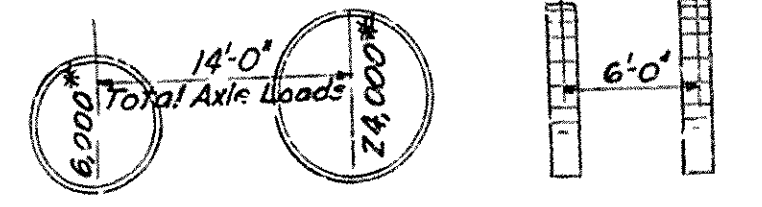


Notes

Rivets $\frac{3}{4}$ " Open holes $\frac{1}{8}$ "
 All holes in truss connections to be sub-punched and reamed to size while truss is assembled; this applies to field as well as shop rivets.
 Floor beam connections to be sub-punched and reamed to a $\frac{1}{8}$ " template.
 All field connections shall be riveted.
 Shop paint: After being completely assembled and shop work finished, all pieces shall be given one coat of red lead and raw linseed oil before shipment.
 Field paint: Apply two coats of different colors as specified by the engineer.
 Floor slab: Concrete to be Class "S". One inch has been added for wear.
 All floor beams to be milled to exact length after framing angles have been riveted.
 Shapes of equal or greater strength may be substituted for shapes shown, but payment will be made in accordance with sizes given on this plan.
 This drawing shows general features of design only.
 Shop drawings shall be made in compliance with specifications, submitted and approved before fabrication is begun.
 Specifications: Ark Standard Road & Bridge Specifications, adopted May 30, 1925 & Revised.

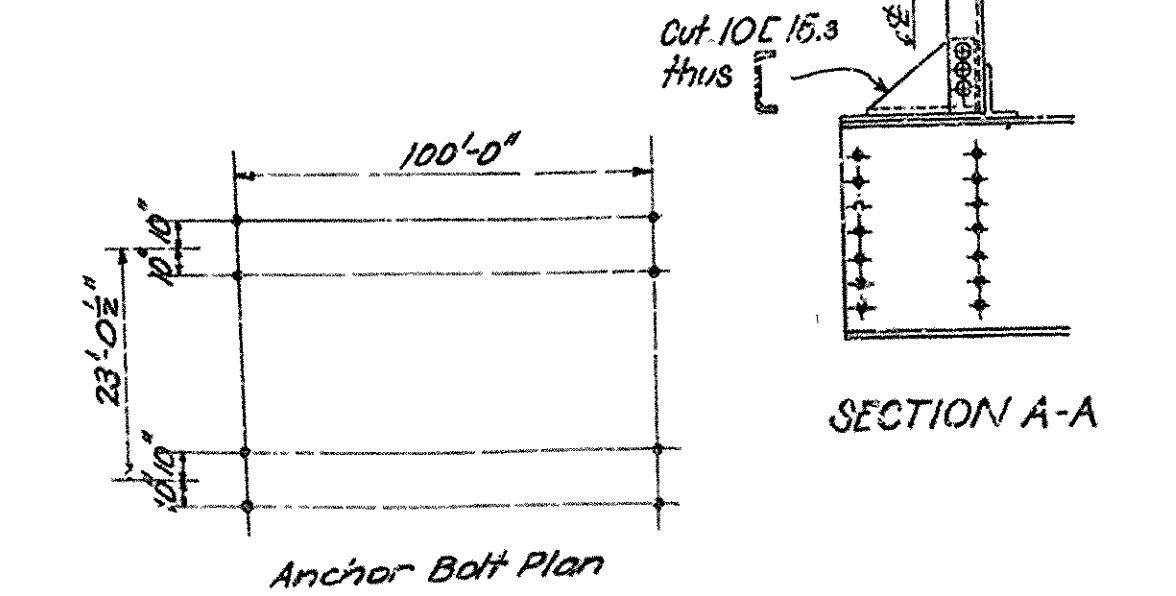
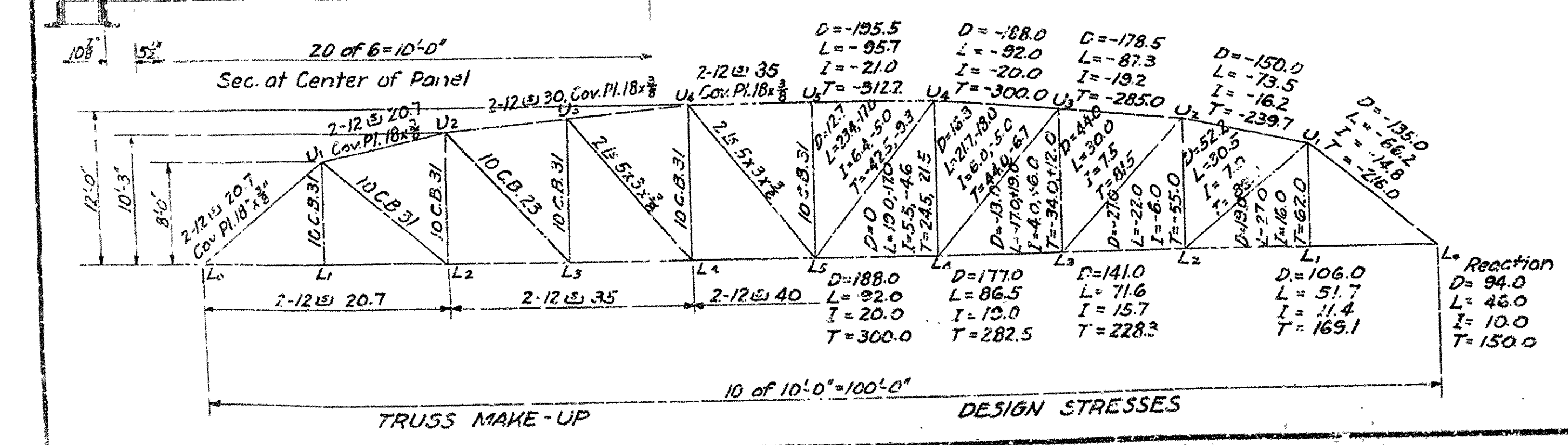
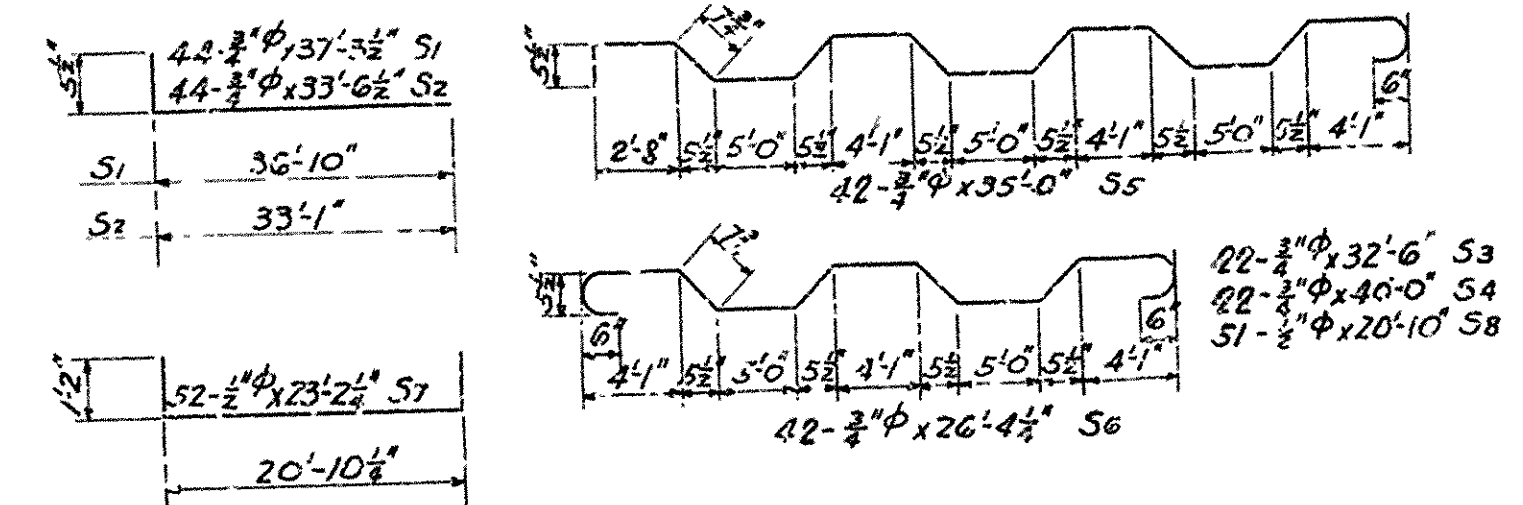
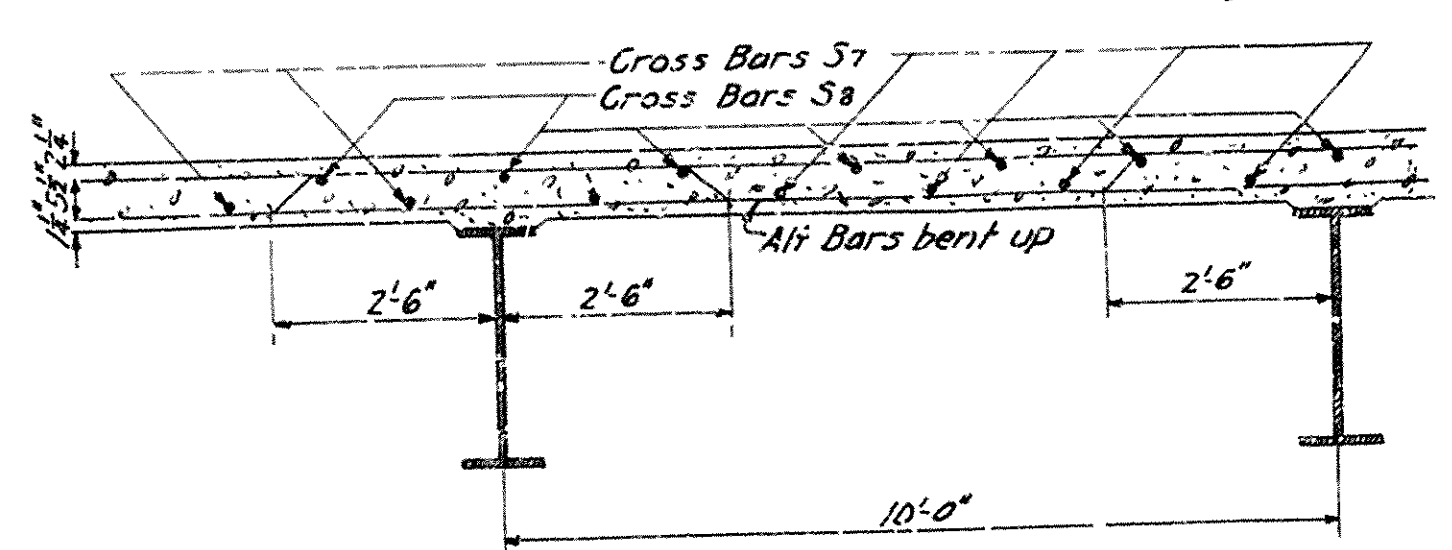
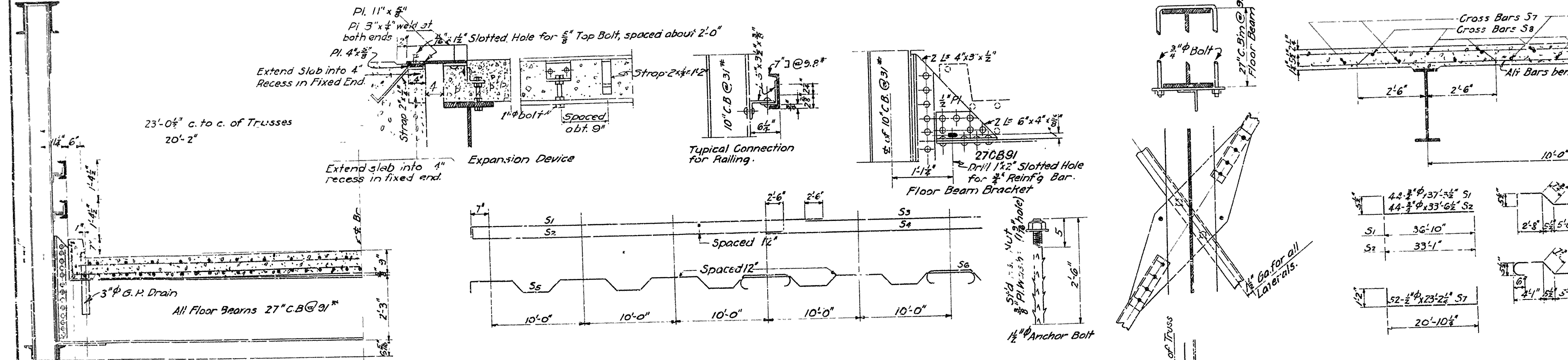
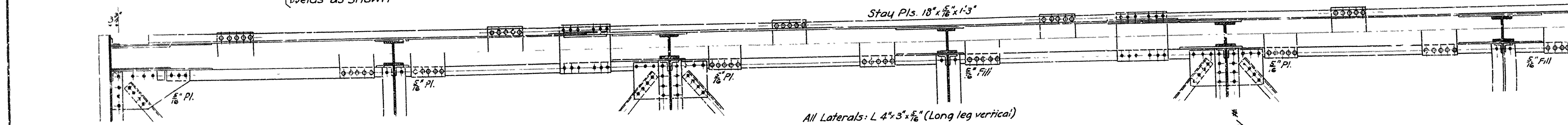
Unit Stresses

Concrete 750# per sq. in.
 Reinforcing Steel 16,000#
 Structural Steel 16,000#
 Live Load His Loading



Estimated Quantities

Structural Steel 85400 lbs
 Cast Steel 1670 "
 Reinforcing Steel 12480 "
 Concrete Class "S" 62.4 cu yds



Revised Expansion Device 9-11-30 Anderson.

Revised March 6, 1931
 Raymond Deville and
 Anderson
 by Anderson

STANDARD PLAN
 100'-0" LOW TRUSS SPAN
 20'-0" CLEAR RDWY
 ROUTE 300

ARKANSAS STATE HIGHWAY COMMISSION
 LITTLE ROCK, ARK.

Drawn By: A Date: 1-30-30 Scale: 1/2" = 1'-0"
 Traced By: [Signature] Date: 1-30-30
 Checked By: [Signature] Date: [Blank]
 BRIDGE ENGINEER
 BRIDGE NO. DRAWING NO. 2418